

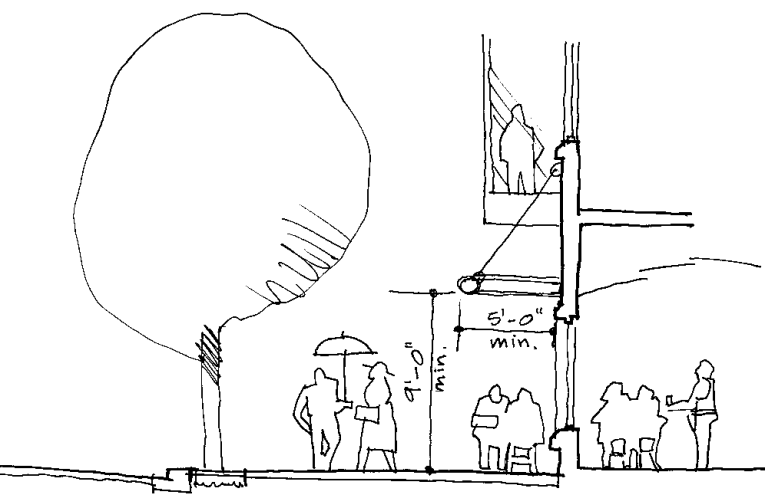


City of Vancouver Land Use and Development Policies and Guidelines

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COMMERCIAL STATION CD-1 GUIDELINES

Adopted by City Council June 13, 2000



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Note: These guidelines are organized under standard headings. As a consequence, there are gaps in the numbering sequence where no guidelines apply.

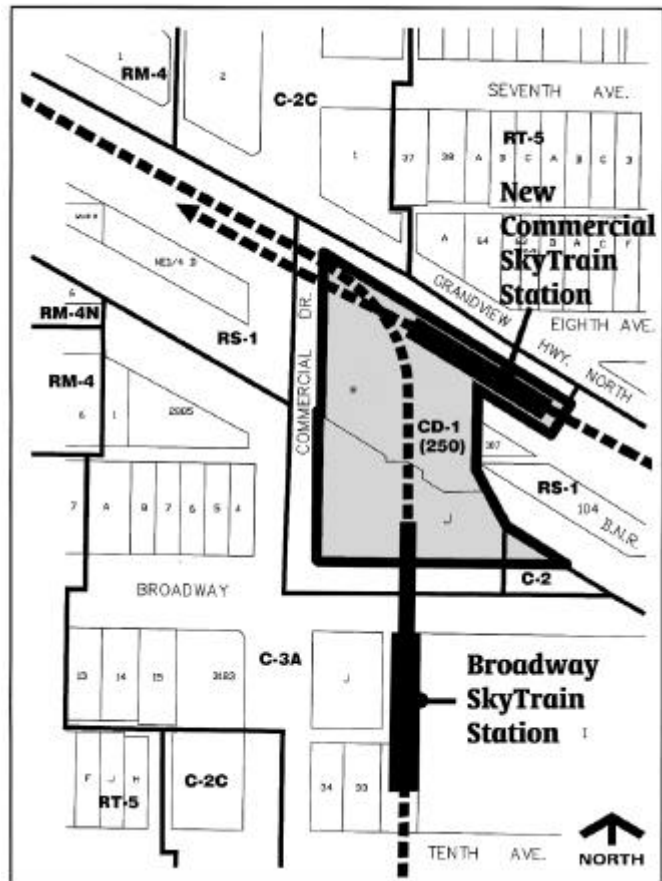
1 Application and Intent

As well as assisting the applicant, the guidelines will be used by City staff in the evaluation of projects.

The intent of the guidelines is to:

- (a) Achieve a high quality, safe and vibrant station and transit interchange with mixed use development at the northeast corner of Broadway and Commercial Drive.
- (b) Strengthen Commercial Drive as a core shopping and business area, providing continuity of retail development, integrated pedestrian spaces and enhanced streetscapes.

Figure 1. Proposed Commercial Station CD-1 Location



2 General Design Considerations

The existing retail area along Commercial Drive and Broadway is composed of pedestrian-related grade level retail uses with offices above in some cases. While many retail activities depend on local support, the presence of three medical buildings plus one of the largest Safeway stores in Vancouver, recently renovated, combine to anchor the area and draw many people from the larger community. Much recent development has occurred on the east side of Commercial Drive between 10th and 12th Avenues. The six-storey Eastvan Medical Building is located on 10th Avenue just east of Commercial Drive. The most recent mixed-use development is located on the southwest corner of Broadway and Commercial

Physical changes should enhance the appearance and character of the street as a shopping area and contribute in creating a stronger visual image for the station precinct. Such features as storefront awnings and canopies providing rain protection, display windows, fascia-type signage, individuality of shop frontages and a high quality of architectural design and streetscape amenity are encouraged.

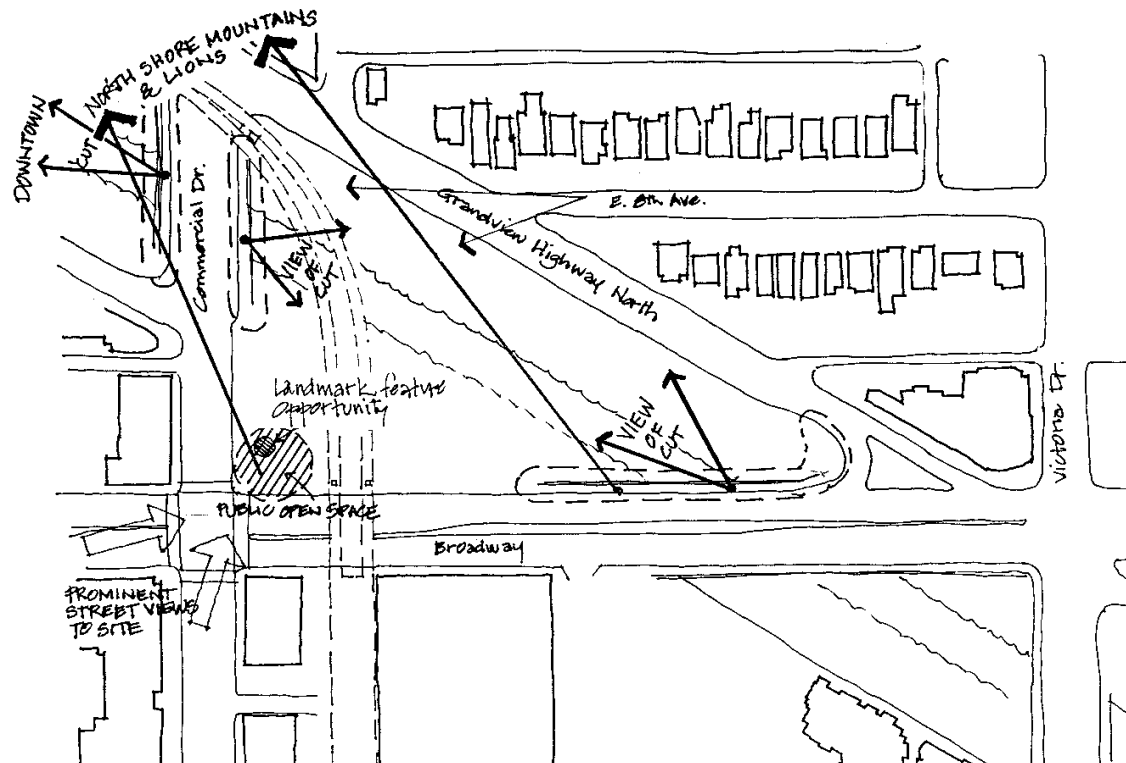
The Commercial Drive Station, in combination with the existing Broadway Station to which it is linked, could provide the catalyst for transit-related and pedestrian-friendly development on corner sites at the Broadway/Commercial Drive intersection. The corners should become focal points for the community and major activity nodes. New development should express the importance of the corner by being higher than the surrounding area.

2.4 Views

Good views to the downtown and North Shore mountains exist in the area, and should be maintained from important public vantage points. The Broadway/Commercial Drive intersection becomes a focal point when approaching along Commercial Drive from the south and along Broadway from the west.

- New development at the intersection should acknowledge the prominence of the site and its role as the heart of the neighbourhood and create a focal point visible at a distance from the Stations.
- Distant vistas along the axis of the Cut toward the Downtown skyline from the Commercial Drive Bridge and the Broadway Bridge.
- Intermediate views into the Grandview Cut should be maintained from the Commercial Drive Bridge and the Broadway Bridge.
- Views into the Grandview Cut should be available from major public circulation and amenity areas within the station and station house.

Figure 2. Views to be protected



2.5 Topography

The Grandview Cut will be significantly altered on the north embankment by the construction of the guideway, retaining walls and the new Commercial Station, and on the south side where the new commercial development and station house are located. A “green” design and impact mitigation approach with respect to retaining walls design, revegetation and maintenance is encouraged.

- (a) Exposed retaining wall surfaces should be fenced off to deter public access in order to minimize exposure to hazards and defacement.
- (b) Exposed retaining walls should be designed to provide areas for permanent re-landscaping with creepers and vines.

2.7 Weather

Transparent or translucent weather protection should be provided for retail frontages, and be of sufficient depth and height to protect pedestrians from wind-driven rain..

2.8 Noise

The decision to locate the guideway below street level in the Grandview Cut is preferable to an elevated structure, from the point of view of noise exposure of nearby residences. However, some residential developments adjacent to the Grandview Cut may still be exposed to noise impacts from the system. Appropriate measures should be undertaken to reduce or mitigate such impacts, as close to the source (wheels and tracks) as possible.

2.10 Safety and Security

Safety and a sense of personal security are essential components of transit station design, and particularly so at this major transit interchange. New development should take into consideration the following Crime Prevention Through Environmental Design (CPTED) guidelines, having particular regard to reducing opportunities for mischief and vandalism, and increasing personal safety.

- (a) Maximize opportunities for natural surveillance
- (b) Provide unobstructed and transparent sightlines to exits and destinations
- (c) Foster territoriality and a sense of ownership
- (d) No hiding places
- (e) Natural and artificial lighting to all public areas
- (f) Supervised use of amenities, including washrooms, telephone and bicycle lockers.
- (g) Walls and surfaces coated with anti-graffiti coating.

2.11 Access and Circulation

This location will become an even more important transit hub in the future, with the addition of a second SkyTrain line. Transfers between surface and other modes, including walking and bicycling, will need to be accommodated comfortably and efficiently. Thus, multiple choices of pathways should be available to transit users to connect to and from nearby bus stops and sidewalks.

3 Uses

Commercial Drive should be reinforced as a shopping street with local and district shopping uses being the dominant activity in any new ancillary development. Continuous small frontages reflecting historical parcel widths and expressing a variety and diversity of activities for shoppers are appropriate at grade. The proposed Commercial Station will add to the pedestrian loads and waiting areas on sidewalks and at bus stops and entry points. Uses that support the activities of the broadest cross section of the public are considered to be appropriate activities for ancillary development at this location.

3.1 Uses at Grade

- (a) Public Authority Use (SkyTrain Station), retail shops, small restaurants, groceries, public amenities, libraries and service oriented uses such as dry cleaners are encouraged at street level.

3.2 Uses Above or Below Grade

- (a) Generally new office development should be local in character and scale serving the needs of adjacent communities with such tenants as dentists, doctors, lawyers and accountants, and be located above grade.
- (b) Uses serving a wider public, such as cultural, recreational and entertainment uses are also desirable for this location, but should not be located at grade.

Figure 3. Examples of uses - Retail continuity at grade, theatre, office and large restaurant above or below grade



4 Guidelines Pertaining to the Regulations of the Zoning and Development By-law

4.2 Frontage

- (a) New commercial development should incorporate design elements that reinforce an incremental rhythm at the street level. All businesses should provide a continuous retail frontage which will be a benefit to both the pedestrian and merchant.
- (b) Storefronts should be small to encourage window shopping and continuing pedestrian interest.
- (c) Building walls fronting on streets leading to the Station should be articulated to the scale of pedestrians, emphasizing a more traditional vertical expression and proportions. Upper story bay window treatment is encouraged.

4.3 Height

- (a) New development should generally create a two- to three-story streetwall located at the setback line for the Broadway/Commercial intersection and the Commercial Station.
- (b) Height at the streetwall location should generally not exceed 12.2m (40 feet), but the station house may exceed this height in order to reflect the intended transit function.
- (c) A strong projecting cornice line is encouraged to reinforce human scale and enhance facade articulation.
- (d) Stories above grade level may project over the floors below in a manner that reflects a contemporary interpretation of the traditional bay window.

4.4 Front Yard and Setback

Most existing commercial development has no front yard setback. While this creates a cohesive image for the street, the existing sidewalks, particularly at the Broadway/Commercial Drive intersection and on the east side of Commercial Drive are of insufficient width to accommodate the increased number of pedestrians attracted to the existing and new SkyTrain stations in reasonable comfort.

- (a) In order to accommodate increased pedestrian traffic and allow public activity on the sidewalk area, sidewalks should be at least 6.0m in width.
- (b) Retail continuity should be preserved and consideration given to the provision of pedestrian amenities.
- (c) Outdoor extensions of cafes and restaurants are encouraged.

4.9 Off-Street Parking and Loading

Parking and loading are essential service functions. They can also seriously detract from the streetscape of developments with no lanes or sides streets available for access. Further, while the reduction of parking supply is realistic for many uses at or adjacent to a major transit node, the demand does not disappear altogether.

- (a) All off-street parking areas should be provided on-site or in collective parking.
- (b) The impact of parking congestion on any adjacent residential street should be minimized.

5 Architectural Components

5.1 Roofs

New development near the Broadway/Commercial Drive intersection should create interesting and articulated roof forms to create a visually exciting skyline and focus appropriate to the regional significance of this transit hub.

- (a) On sites adjacent to the ALRT guideway, any roof lower than the guideway will become visible to ALRT riders. Roofs should be designed so that they are visually attractive and interesting, as a standard flat tar and gravel roof could easily become unsightly with wear and age. (b) Sloped roof types are considered most appropriate.
- (c) Mechanical equipment should be suitably screened.

Figure 4. Example of a roofline that expresses the transit function



5.2 Windows and Skylights

- (a) Windows at grade are essential to enhance pedestrian experience and provide casual surveillance of both the street and station house areas.
- (b) Large skylights and/or greenhouse-like roofs in public areas are encouraged to provide both daylight and visual connection between pedestrians at various levels and both commercial and SkyTrain facilities.

5.3 Entrances

- (a) New commercial development at grade level should provide entrances no further than 10m apart, that are of a pedestrian scale and which reinforce vertical facade articulation and visual interest while providing weather protection.
- (b) Grade level entries should be oriented to both the fronting sidewalk and the ALRT station circulation areas, in order to provide both convenient pedestrian access and a sense of ownership of adjacent public space.
- (c) Within the front setback areas of Commercial Drive and Broadway, recessed doorways are not encouraged, as they can provide areas difficult for surveillance.

5.5 Exterior Walls and Finishes

- (a) The lower levels of development and station areas should be carefully designed to relate to pedestrian scale, and enhance the close-up view for the pedestrian. The use of quality and durable materials with attention to detail that contribute to pedestrian interest are encouraged.
- (b) Blank walls in public areas of station facilities and commercial development are discouraged.
- (c) Where such walls are necessary, public art, vines and other measures should be used to soften appearance and deter graffiti.
- (d) An anti-graffiti coating should be applied to all wall surfaces.

Figure 5. Example of ground level pedestrian interest and transparency

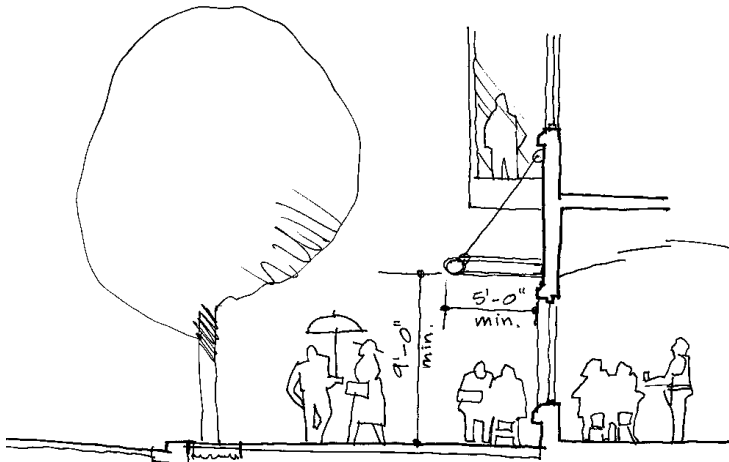


5.6 Projections, Awnings and Canopies

Projecting upper stories and overhangs, awnings and canopies are all desirable from a weather protection aspect. In terms of appearance, using a uniform awning or canopy design across the length of a development is inappropriate to the sense of small scale storefronts intended for the area.

- (a) Design architecturally integrated, high quality transparent or translucent awnings and canopies, but ensure some variety in form.
- (b) Ensure that awnings and canopies are deep enough and close enough to the ground to provide adequate shelter, and without large gaps between them.
- (c) Awnings and/or canopies should have a minimum depth of 5'-0".
- (d) Awnings and/or canopies should have a minimum height of 9'-0".

Figure 6. Awnings and Canopies



5.7 Lighting

A well thought out and integrated lighting plan can make a large positive contribution to the sense of safety and security pedestrians experience at the station and ancillary development.

- (a) Station areas should have improved lighting levels at all times, day and night, especially under the elevated guideways and at supports on Commercial Drive and Broadway.
- (b) Lighting should be sensitive to nearby residential areas. Visible, glaring light sources should be avoided through use of down- and/or up-lights with cutoff shields.
- (c) Full-spectrum “white” light sources are preferred.

6 Internal Design and Facilities

6.1 Internal Circulation

The functional success of the Commercial Station will depend in large measure upon the ability of transit users to find their way between platforms and surrounding bus stops and other destinations.

- (a) The clear widths of pathway and circulation facilities in the station areas themselves should be of sufficient width to accommodate projected long-term patron movements in comfort.
- (b) Conflicts in the circulation needs of transit riders and shoppers, both moving and stationary, should be minimized.
- (c) Space should be provided for pedestrians to move comfortably aside from main travel paths to attend to the needs of small children and people with disabilities.
- (d) Public circulation facilities, including bridges, ramps, stairways, escalators and elevators, should be conveniently and prominently located in public areas, with direct sightlines from surrounding public areas, including sidewalks and stores.
- (e) Enclosures of such circulation elements should be transparent to the maximum extent possible.
- (f) Directional signage should be conveniently located, well-designed and clearly legible.

Figure 7. Example of unobstructed and transparent internal circulation



6.2 Amenity Areas

- (a) Services and amenities including telephones and supervised public washrooms should be available directly from the station house area.
- (b) Public overlook areas into the Grandview Cut and Commercial Station platform should be integrated into site development.
- (c) Secure bike racks and bike storage lockers should be provided in convenient locations under cover for transit users and shoppers, in places where station personnel and passersby can provide natural surveillance.

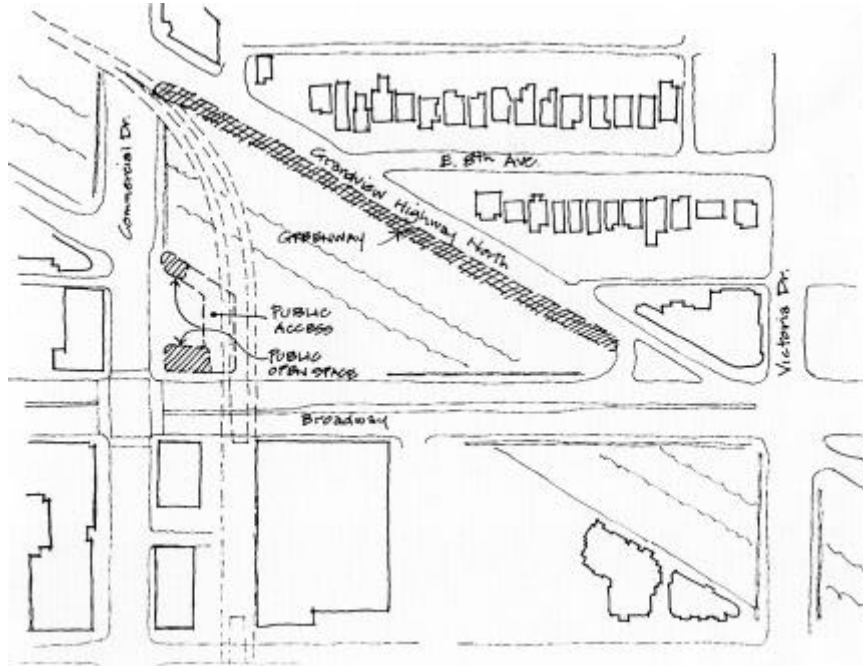
7 Open Space

7.1 Public Open Space

The Broadway/Commercial precinct does not presently enjoy public open space, especially of an urban nature. This site provides a rare opportunity to provide such urban open space in a variety of locations.

- (a) A public open space should be incorporated at the northeast corner of Broadway and Commercial Drive, and should be enhanced with quality soft and hard landscaping, and animated by outdoor extensions of cafes, restaurants and public art.
- (b) Major entry points to the station house should be designed to accommodate landscaping, seating and overlook opportunities for the general public.
- (c) Surfaces, furnishings and fixtures should be designed to deter skateboard use.

Figure 8. Location of public open space opportunities



8 Landscaping

8.1 Streetscape

- (a) New commercial development near the Broadway/Commercial Drive intersection should provide a more urban landscape treatment.
- (b) A bosque or double row of significant street trees should be incorporated in the urban plaza at the northeast corner of Broadway and Commercial Drive, and serve as a landmark feature in this public open space.
- (c) Street trees should be planted along Commercial Drive, Broadway and Grandview Highway North to the satisfaction of the General Manager of Engineering Services.
- (d) Landscape treatment along Grandview Highway North should be designed to help screen the new SkyTrain guideway and Commercial Station from nearby residences.

Figure 9. Example of streetscape with mature street trees



8.2 Site Landscape

- (a) The Grandview Highway North edge of the site should be designed as an integral part of the Central Valley Greenway and Bikeway, and be landscaped such that a protective buffer screens the guideway and station from nearby residences on Grandview Highway North.
- (b) The landscape plan for the Grandview Cut should be consistent with Canadian Environmental Assessment Act requirements for the SkyTrain Extension, to the satisfaction of the Director of Planning.

9 Utilities, Sanitation and Public services

9.2 Underground Wiring

In order to improve the visual environment for pedestrians, transit riders and nearby residents, the development should investigate with the City Engineer the feasibility of using underground wiring for electric, telephone and cable services, including the removal or partial removal of existing overhead utilities.

Submission Requirements

Applicants should refer to the information required for significant development permit applications contained in the Checklist in Brochure #3 - **How To... Development Permits for Major Applications**.