



# BRIDGEHEAD GUIDELINES

[FORMALLY ADOPTED AS “GUIDELINES FOR SITING AND MASSING BUILDINGS ADJACENT TO DOWNTOWN VANCOUVER BRIDGEHEADS”]

*Adopted by City Council December 9, 1997*

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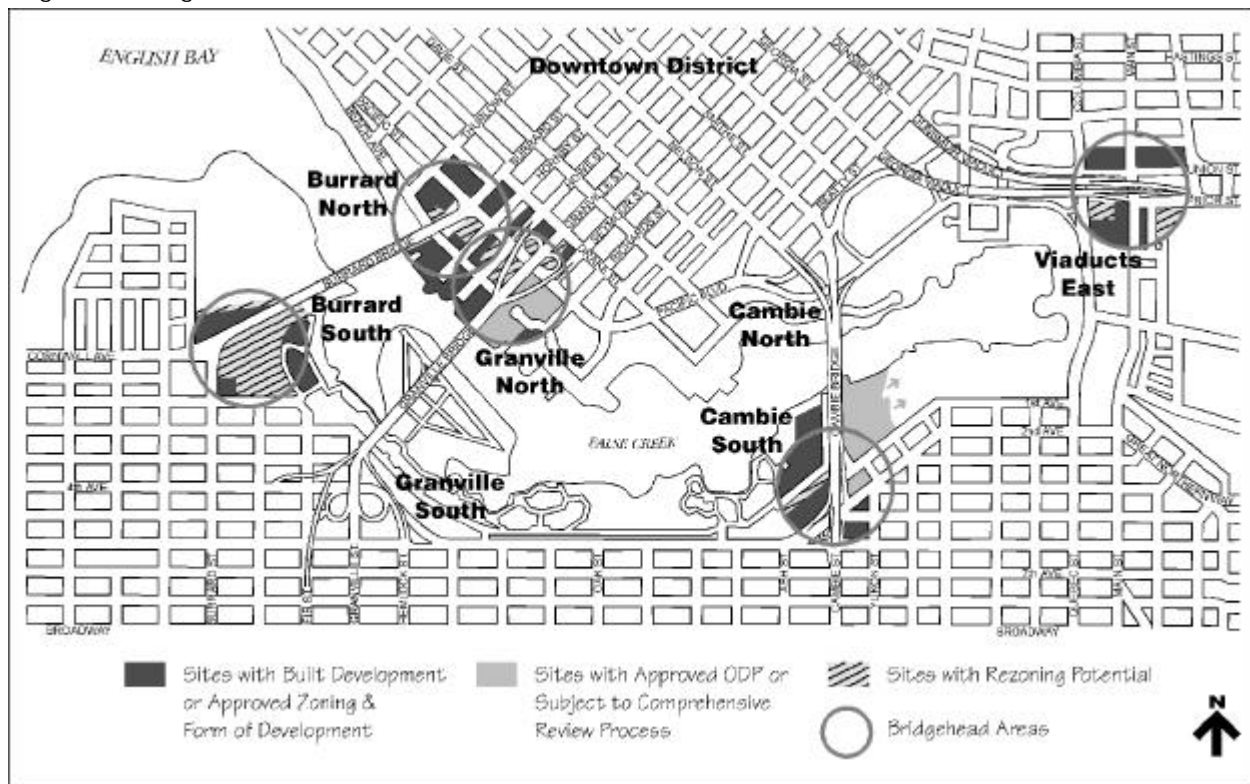
# 1 Application and Intent

These guidelines are to be used in conjunction with the RT-2, RM-5A, C-3A, FC-1, HA-1A, M-1, M-2 and I-2 District Schedules and the False Creek Official and Area Development Plans, the False Creek North Official Development Plan and applicable CD-1 By-laws. These guidelines should be consulted in seeking approval for conditional uses or for the relaxation of regulations. As well as assisting the applicant, the guidelines will be used by City staff in the evaluation of development initiatives and rezoning applications for sites adjacent to selected bridgeheads (see Figure 1).

The intent of these guidelines is to:

- maintain key public views from the bridges;
- reinforce and enhance the experience of crossing the bridges;
- reinforce and enhance existing urban form patterns;
- establish optimum setbacks of towers from the bridge decks;
- limit building height immediately adjacent to the bridges to below the bridge deck;
- minimize views of unsightly roofs from bridges;
- encourage, where possible, improved pedestrian connections; and
- reconcile public objectives with adjacent private development rights and expectations.

Figure 1. Bridgehead Areas



## 4 Guidelines Pertaining to the Regulations of the Zoning and Development By-law, the False Creek Official and Area Development Plans, the False Creek North Official Development Plan and Applicable CD-1 By-laws (NOTE: All setbacks are measured horizontally.)

### 4.1 North Burrard Bridgehead — West Side

The following siting and height guidelines (see Figure 2) should be followed for buildings that are proposed on the north Burrard Bridgehead (West Side):

- (a) no buildings should be within 10 m of the bridge deck;

- (b) between 10 m and 30 m, the building should not exceed 4 storeys (except as noted in (d) below);
- (c) buildings exceeding the height of the bridge deck should be setback a minimum of 30 m from the drip line of the bridge; and
- (d) a limited portion of the building face opposite the bridge (no more than one-third of the depth of the building) may extend up to 3.1 m into the setback area to provide articulation, reduce the sense of scale of the building from the bridge and contribute to a more efficient floorplate.

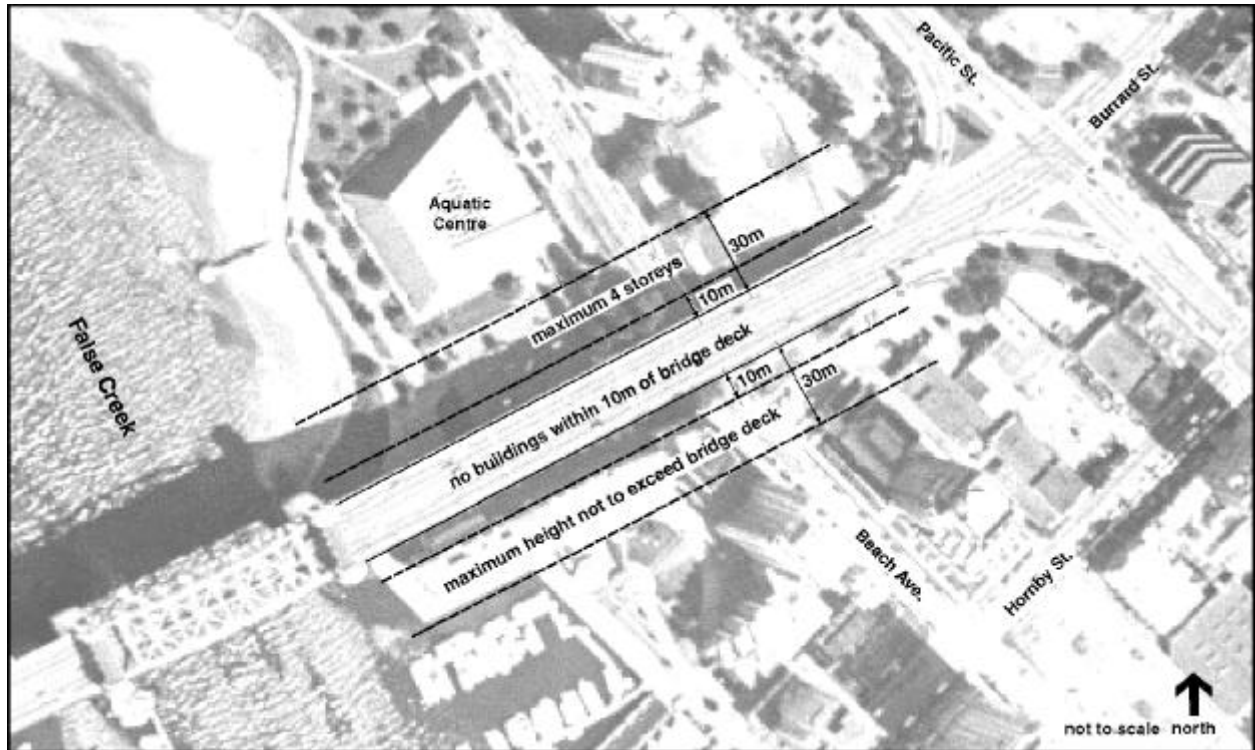
In addition, consideration should be give to the provision of a footpath along the westerly edge of the bridge.

#### 4.2 North Burrard Bridgehead — East Side

The following siting and height guidelines (see Figure 2) should be followed for buildings that are proposed on the north Burrard Bridgehead (East Side):

- (a) no buildings should be within 10 m of the bridge deck;
- (b) between 10 m and 30 m, buildings should not exceed the height of the bridge deck (including elevator penthouses); and
- (c) buildings exceeding the height of the bridge deck should be setback a minimum of 30 m from the drip line of the bridge.

Figure 2. Burrard Bridge North — Guidelines for development adjacent to the bridgehead



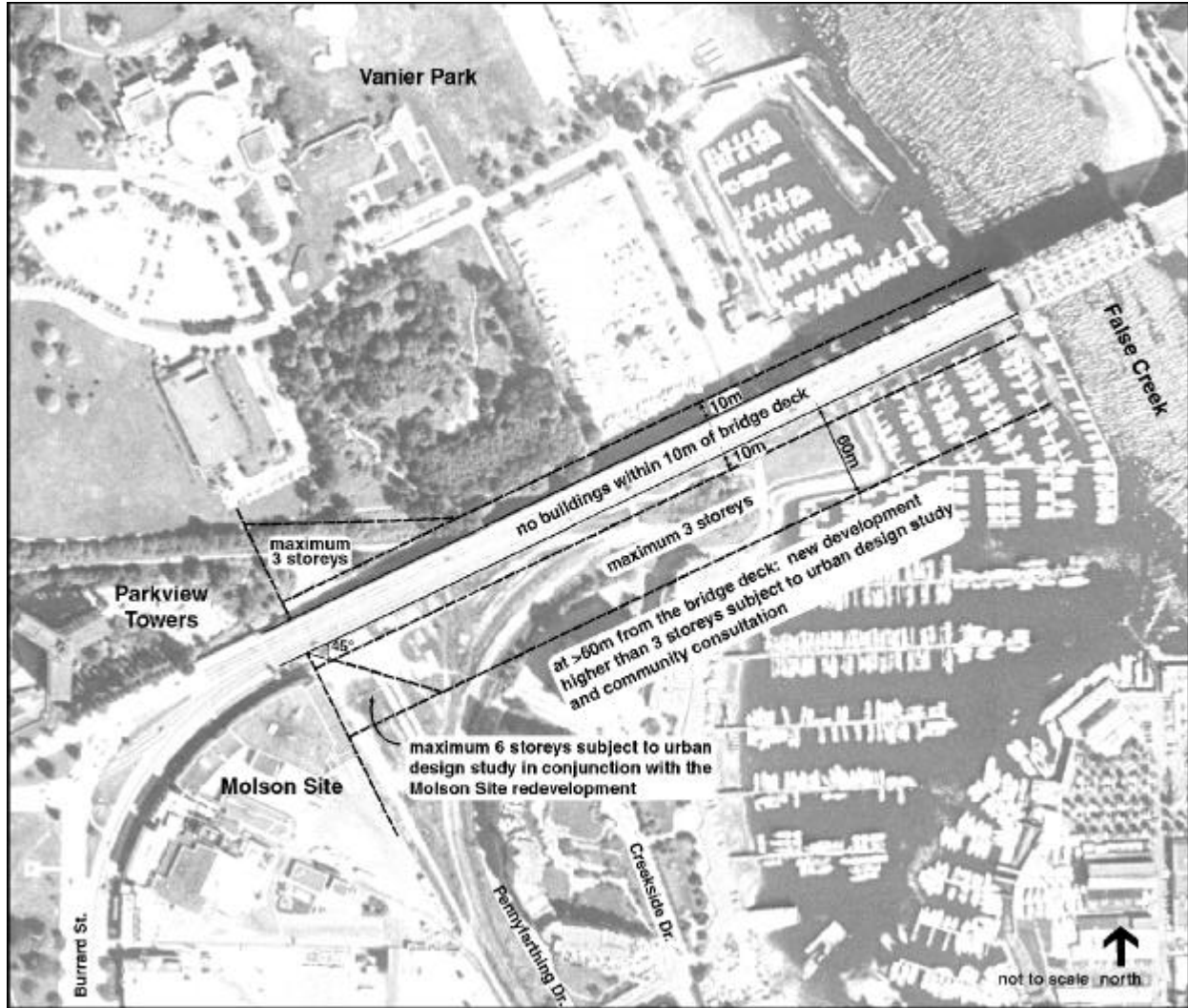
#### 4.3 South Burrard Bridgehead — East and West Sides

The following siting and height guidelines (see Figure 3) should be followed for buildings that are proposed on the south Burrard Bridgehead (East and West Sides):

- (a) between the bridge deck and 10 m — there should be no buildings;
- (b) east of the bridge, between 10 m and 60 m — maximum 3 storeys with higher buildings possible immediately adjacent to Molsons Brewery (see Figure 3); and
- (c) east of the bridge, higher buildings are permitted beyond the 60 m setback but their exact height to be subject to a comprehensive urban design study of the area and further community consultation; and

- (d) west of the bridge, maximum of 3 storeys to minimize shadowing of Vanier Park.

Figure 3. South Burrard Bridge — Guidelines for development adjacent to the bridgehead



#### 4.4 North Granville Bridgehead — West Side

The following siting and height guidelines (see Figure 4) should be followed for buildings that are proposed on the south Granville Bridgehead (West Side):

- (a) no buildings should be within 10 m of the bridge deck;
- (b) between 10 m and 30 m, building should not exceed the height of the bridge deck (including elevator penthouses); and
- (c) buildings exceeding the height of the bridge deck should be setback a minimum of 30 m from the drip line of the bridge.

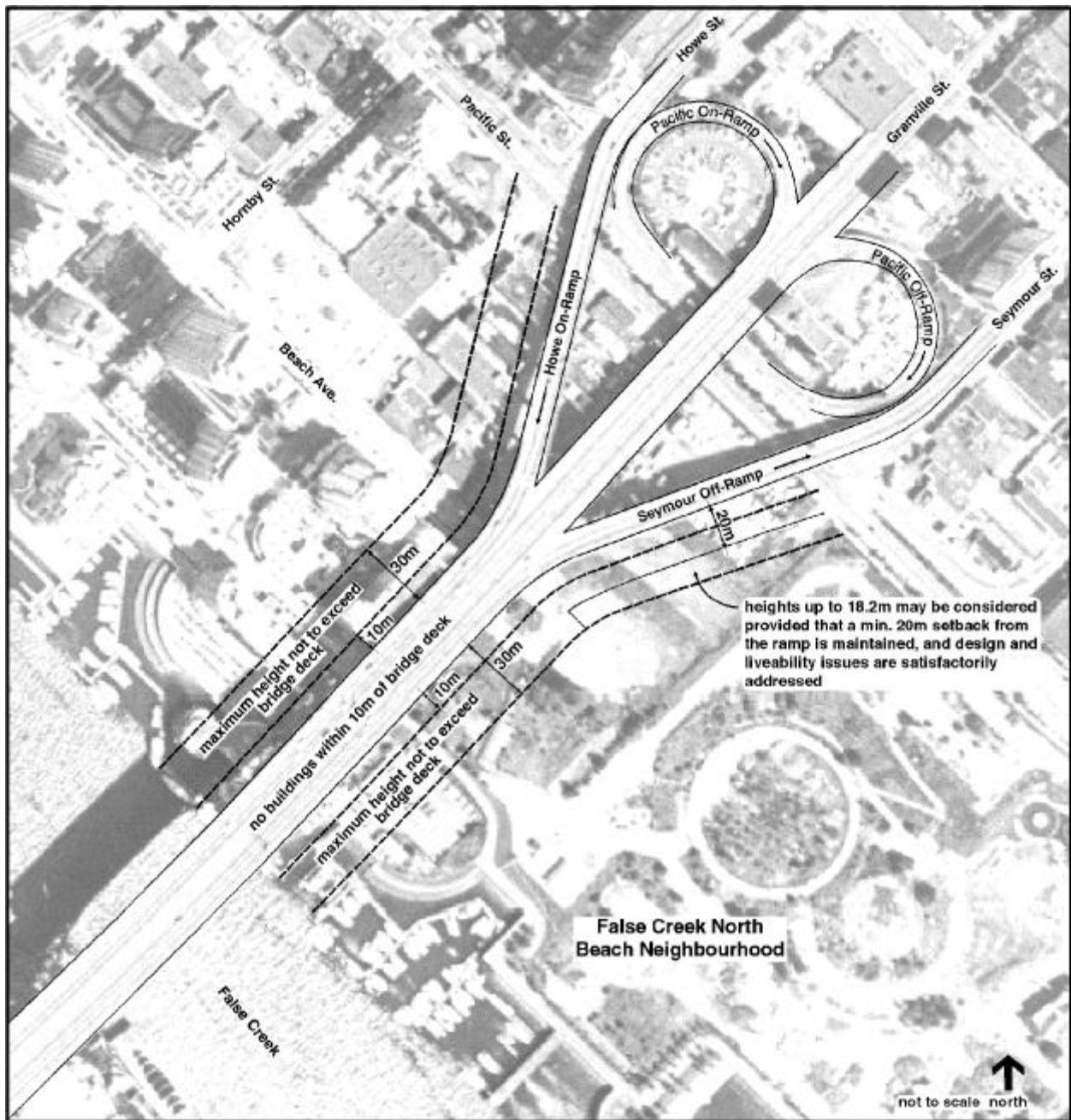
#### 4.5 North Granville Bridgehead — East Side

The following siting and height guidelines (see Figure 4) should be followed for buildings that are proposed on the north Granville Bridgehead (East Side):

- (a) no buildings should be within 10 m of the bridge deck;
- (b) between 10 m and 30 m, building should not exceed the height of the bridge deck (except for sites adjacent to the Seymour ramp); and
- (c) for sites east of the Seymour ramp, buildings up to 18.2 m in height may be considered between Pacific Street and Beach Avenue provided that:

- a 20 m minimum setback is maintained;
- the roof is positively articulated as a visible elevation(eg. sloping roofs, gables, dormers, bay windows, cupolas, etc.); and
- livability issues are satisfactorily addressed.

Figure 4. Granville Bridge North — Guidelines for development adjacent to the bridgehead



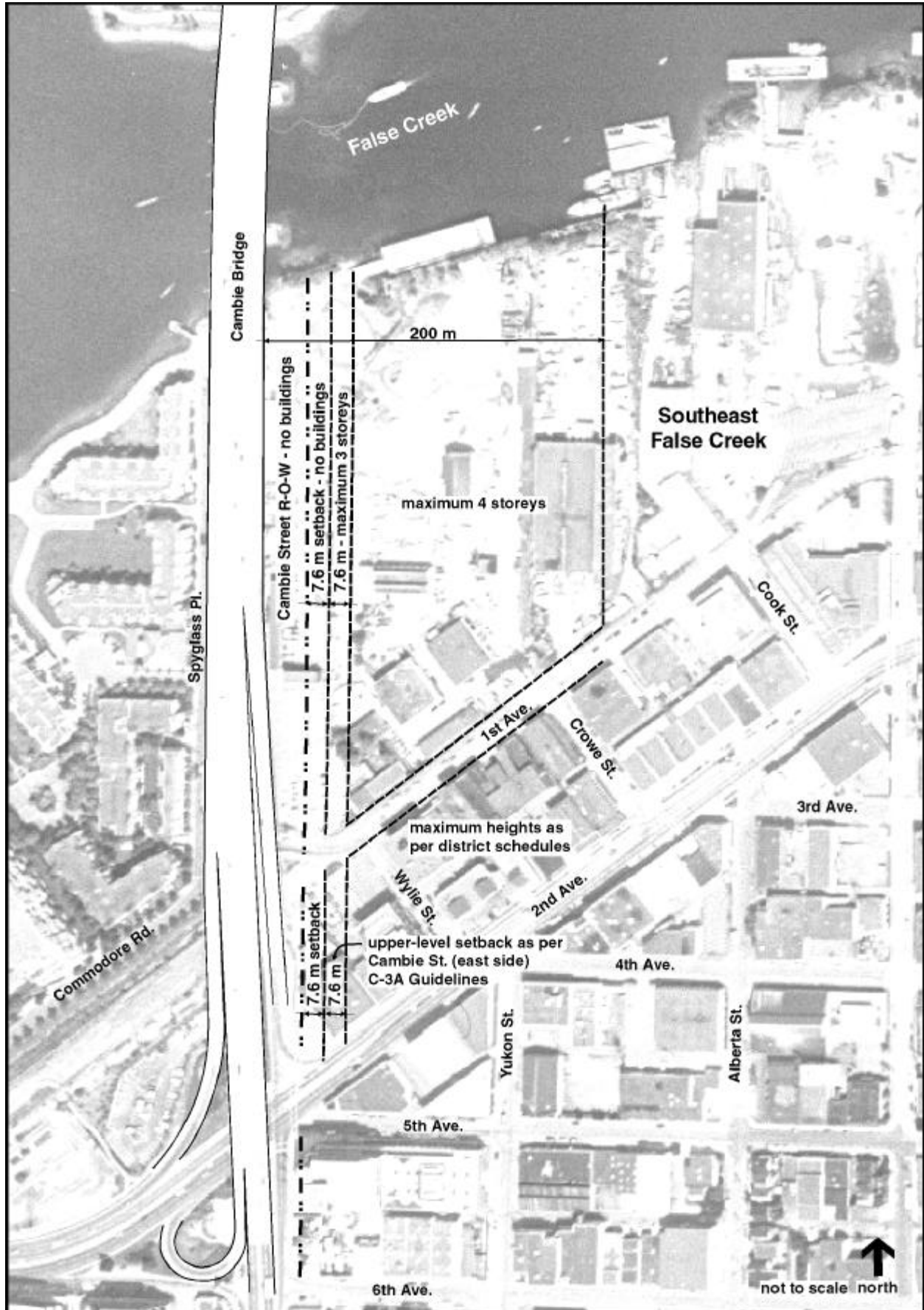
#### 4.6 South Cambie Bridgehead — East Side

The following siting and height guidelines (see Figure 5) should be followed for buildings that are proposed on the south Cambie Bridgehead (East Side):

- north of 1st Avenue, no buildings should be permitted between the bridge deck and 7.6 m east of the eastern property line of Lot 56, D.L. 2064, Plan 5568 (Cambie Street R.O.W.), to the water;
- north of 1st Avenue, the openness of this area should be retained by limiting building heights as follows:

- (i) between 7.6 m and 15.2 m east of the eastern property line of Lot 56, D.L. 2064, Plan 5568, buildings should not exceed 3 storeys;
- (ii) between 15.2 m east of the eastern property line of Lot 56, D.L. 2064, Plan 5568 and 200.0 m east of the bridge deck, buildings should not exceed 4 storeys;
- (c) south of 1st Avenue, the Cambie Street (East Side) C-3A guidelines should be consulted for determining lower and higher building elements within these prescribed setbacks.

Figure 5. Cambie Bridge South — Guidelines for development adjacent to the bridgehead (east side)



#### 4.7 Georgia Viaducts East

The following siting and height guidelines (see Figure 6) should be followed for buildings that are proposed on sites adjacent to the Georgia Viaducts East:

- (a) a maximum building height of 55 m should be permitted for the Greyhound site west of Main Street, subject to detailed planning review;
- (b) a maximum building height of 23 m should be permitted on the site at the southeast corner of Prior and Station, with an increase in height of up to 30 m permitted at the corner forming the visual terminus of the view down the Main Street off-ramp, provided this corner element is treated as an appropriate feature; and
- (c) a maximum building height of 23 m should be permitted on all other sites immediately adjacent to the viaduct and off-ramps to Main Street.

Figure 6. Georgia/Dunsmuir Viaducts — Guidelines for development adjacent to the viaduct ramps

